



Bardstown Road Baxter Avenue Overlay District Guidelines

Principles and Guidelines

Ordinance No. 69 Series, 2009

OBJECTIVE

The Bardstown Road / Baxter Avenue Overlay District (or BROD, for short) was created in January 1990 to help enhance the appearance and economic vitality of one of the community's most successful and appealing neighborhood commercial areas- The stretch of Baxter Avenue and Bardstown Road running through the Highlands. BROD is administered by the city's Division of Planning and Design Services, with input from a citizen-review panel known as the Bardstown Road / Baxter Avenue Corridor Overlay Review Committee. Within the Division of Planning and Design Services the city's Urban Design Administrator and Staff (Overlay Staff) administers, manages, and conducts the architectural review process.

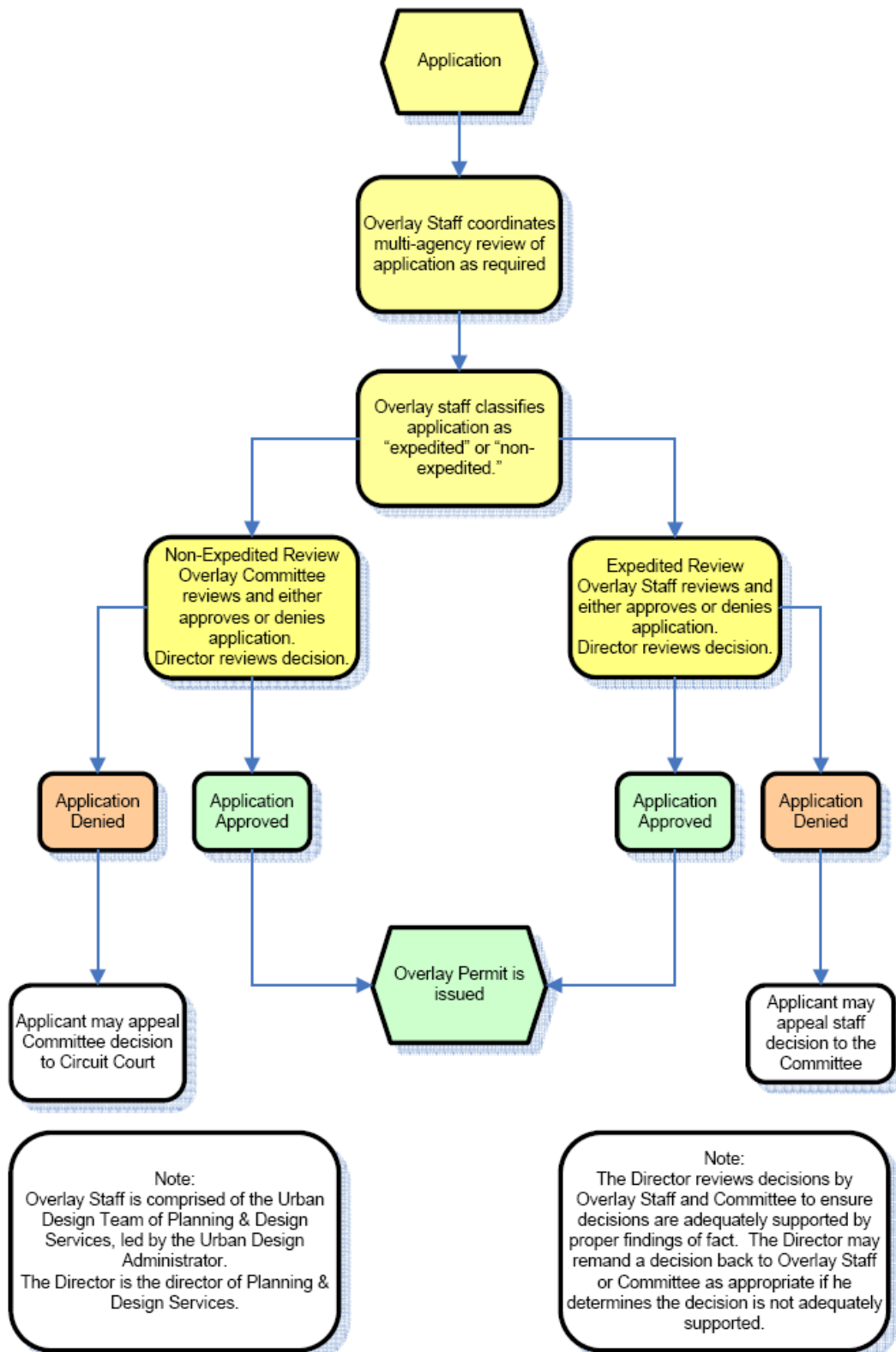
The guidelines are not intended to discourage development or to dictate architectural design or style, but to encourage such development that contributes to the overall design quality of the Bardstown Road Baxter Avenue Overlay District. The guidelines address construction and other external changes to buildings and properties located in the Overlay District.

They are intended to promote compatibility of new development with existing land use and design features, to enhance the Bardstown Road Baxter Avenue's visual quality, to preserve the District's commercial character with a pedestrian friendly environment and to strengthen the economic vitality of District Overlay by encouraging new investment and further business, residential, and commercial development in appropriate locations in Downtown.

All development proposals within the Overlay shall comply with applicable guidelines however a proposal that does not conform to one or more specific guidelines may be approved by the Committee or its staff if they determine that the proposal is in conformance with the intent of the guidelines as a whole.

The principles and guidelines of the Bardstown Road Baxter Avenue Overlay District expand upon the requirements of the Land Development Code as necessary to achieve the objectives of these facets of urban design in recognition of a project's specific location.

**BARDSTOWN ROAD / BAXTER AVENUE CORRIDOR
REVIEW OVERLAY DISTRICT
-Review Process-**



Signage

Objective

Signage is important to every business. By posting signs you not only create a brand image for your business, but also create an opportunity to attract customers. The Signage Guidelines promote attractive, artistic, well proportioned and thoughtfully located signs which will enhance the existing Bardstown Road / Baxter Avenue Corridor. Signs are to be designed as integral parts of the site and architectural design of a proposed project.

Sign Design Guidelines

1. Applicants are required to re-use and reface existing "significant or contributing signs". "Significant or Contributing Signs" are signs that are unique in design or material fabrication. These signs contribute to the history and diverse character of the BROD. (See list of significantly contributing Signs; next page). For signage to be recognized as "significantly contributing" or for existing recognized signs to be removed or altered an application must be submitted to the overlay staff.
2. Contributing signs which have been painted directly onto walls and represent out of date logos or logos for businesses that are no longer in business are encouraged to remain in either their existing faded condition or restored to their original condition. These signs are recognized as contributing to the historic artistic character of the corridor and our community.
3. Back-lit cabinet signs and monument signs including re-faced signs shall have opaque, non-illuminated face panels; only the individual letters and / or logos should be back-lit, not the entire surface of the sign.
4. Freestanding pole signs are not allowed. Existing pole signs that are associated with a property that has not been abandoned or vacant for a period of less than (1) year may be restored or refaced.
5. Signs that incorporate flashing text, animation, moving graphics, video, or LED text / image signage with a text change rate greater than (1) hour are prohibited.
6. Storefront window signage including temporary window signage and promotional advertising shall not exceed 25% of the total window area. Neon and hanging window signs are encouraged, and add to the "active atmosphere" for both vehicles and pedestrians.
7. Projecting wall sign can be erected or attached to a building's wall and extend a minimum of 18" to a maximum of 8'-0" from the wall surface. These signs are mounted perpendicular to the building's façade.
8. Projecting banner signs can project no more than 24" from the face of the

building. These signs should be attached so that they do not “flap” in the air, and should be removed or replaced when they show signs of unattractive wear.

9. Attached wall signs or projecting signs are preferred. Attached signage composed of individual letters or symbols is also preferred.

10. Monument Sign: Monument signs are allowed only for businesses with a minimum front yard of 15'-0" as measured from the street to the wall of the structure. Monument signs must also be set back a minimum of 15'-0" from the adjacent side property line. For businesses with less than the required setback requirements; attached wall, awning, or window signage is recommended.

11. Existing non conforming signage must be removed prior to new approved signs being installed. A “Significant or Contributing Sign” as defined by the guideline in A1.1 is exempt from this requirement.

12. Neon and Neon simulated bulbs such as (“fiber optic” and “LED lighting strips” that simulate neon) are encouraged and help to maintain a sense of activity and contribute to the animated environment of the corridor.

13. Projecting lighting fixtures used for externally illuminated signs shall be simple and unobtrusive in appearance. They should not obscure the graphics of the sign.

14. Directional signage is used to direct pedestrian or vehicular traffic. No more than (1) image, logo, or text combined with the “entry” or “exit” text is allowed on each directional sign. Directional signage can not exceed 3'-0” in height above finished grade.

15. All exposed conduit, electrical transformer boxes, and electrical raceways should be concealed from public view, or painted to blend in with background.

LIST OF CONTRIBUTING SIGNS

* Doo Wop Shop	1587 Bardstown Rd.
* Leatherhead	1601 Bardstown Rd.
* Cricket	1707 Bardstown Rd.
* Twig and Leaf	2122 Bardstown Rd.
* Tom Drexler Plumbing	2233 Bardstown Rd.
* Jarfi's	1543 Bardstown Rd.
* Old Town Liquors	1529 Bardstown Rd.
* Sonotone	1839 Tyler Parkway
* Rose	2307 Bardstown Rd.
* Buffalo Wild Wings	1055 Bardstown Rd.
* Horton's Porter Hardware	2202 Dundee Rd.
* Falls City Beer Sign for Outlook Inn	916 Baxter Ave.

Sign Mounting and Placement

Objective

The following signage guidelines will help you bring greater awareness to your business while allowing signs to add to the diversity and character of the corridor.

Sign Mounting and Placement

1. Signs shall be mounted or erected so they do not obscure the architectural features or openings of a building.
2. Signs may not be located in the right-of-way unless they are approved by the public works department. Exceptions include portable "A" Frame Signs which must be removed at the close of the business day. "A" frame signs can not exceed 4'-0" in height and 3'-0" in width.
3. All wood signs need to be stained or painted and have a finished appearance.
4. No sign or portion of a sign shall extend above the cornice line at the top of the building facade or exceed 20 feet in height above finished grade. Rooftop signs are prohibited.
5. New outdoor advertising billboards are not permitted. Removal of existing billboards is encouraged to promote an active and engaging atmosphere for pedestrians and vehicle traffic.

Awning Guidelines

Objective

Awnings can add aesthetic value as well as provide shade and cover for pedestrians. There are however a few guidelines that need to be followed. Awnings and canopies should be mounted in locations that respect the design of a building and the neighboring structures; including the arrangement of bays and openings. In general, they should not obscure transom windows, grillwork, piers, pilasters, or ornamental features of a structure.

Awning Guidelines

1. Lettering, logos, and other symbols shall take up less than 33% of the total area of an awning that is attached to a commercial structure. It is recommended that signs and logos be located on the valance area of awnings that have them.

2. Convex or (bull nose) awnings are not allowed. Shed awnings are visually lighter and have simpler features, and they are more traditional in appearance than convex or bull nose awnings. Awnings with no end panels are more transparent and allow better views into openings and storefronts. Awnings with back-lit graphics or other kinds of interior illumination are not allowed.
3. Approved awning fabric materials include canvas and vinyl.
4. Metal or glass canopies may be appropriate on some buildings if they are compatible with the design and scale of the building.
5. Awnings and canopies are to be installed at a minimum of 8'-0" above finished grade so that pedestrian entry clearances are operationally functional. Awnings must project a minimum of 24" from the building. They should be mounted on the wood or metal framing within a door or window opening (and not on the wall surrounding the opening).
6. In openings with transoms, the awnings should be mounted on the horizontal framing element separating the storefront window from the transom. Awnings should be designed to project over individual window and door openings and not be a continuous feature extending over masonry piers or arches.

Building Guidelines

Objective

The buildings along Bardstown Road are not only picturesque but also historic in many ways. New structures should have a "contextual fit" and reinforce the existing pattern of individual storefronts facing Bardstown Road / Baxter Avenue. Contextual design elements include building setbacks, building heights, building form, rhythm of openings, rhythm of horizontal building lines, color, materials, texture, adjacent building styles, and building details should be respected in new projects.

Building Guidelines

1. Existing structures along the Bardstown Road / Baxter Avenue corridor are encouraged to be renovated and reused. The Overlay Staff can assist a licensed Architect or design professional to develop designs that adaptively reuse these structures to meet the needs of new businesses and services. The Overlay Staff will also assist the applicant through the review and approval process.
2. Buildings should be "pedestrian-friendly". Design building façade elements that promote a pedestrian-friendly environment include; building to the edge of sidewalk, large storefront window openings at the ground floor, awnings, canopies, and lighting.
3. All storefront windows and doors at ground level shall have clear glass or light

window tinting. Severe window tinting or mirrored glass is not permitted unless pre-approved by staff for “special conditions”. Examples of “special conditions” may include restaurant kitchen areas, storage space, and restrooms that would need to be hidden from public view.

4. New structures should be located at the front property line. Building sites should provide side yards wide enough to allow for maintenance of the building unless common party walls are provided on the lot line.

5. High quality materials and historically appropriate architectural details at the ground floor / street level of buildings can both accent buildings, and provide visual interest for pedestrians and motorists.

6. New structures greater than three stories high may be permissible if taller portions are set back from the street frontage so that overall sight lines are compatible, and if the increased height is not intrusive towards adjacent structures.

7. A visual terminus, such as a cornice at the top of a wall helps articulate the architecture, and gives it a completed finished look.

8. Roof forms that are inconsistent with the character of the Bardstown Road / Baxter Avenue Corridor include: single pitch (shed) roofs, curving roofs. Flat roof forms with parapets are well-suited to the character and image of the Bardstown Road / Baxter Avenue Corridor.

9. Outdoor eating or temporary seating located within public sidewalk areas must receive staff approval prior to installation. A 4'-0" wide pedestrian zone is required in the public “Right of Way” sidewalk area.

10. All new mechanical equipment that is visible from a public right of way should be installed to have a minimal impact on adjacent properties and from public view. Replacement of existing mechanical equipment is considered general maintenance and will not require a staff review. Additional permits and approvals by other government agencies or authorities may be required.

Public Art Guidelines

Objective

Public art is the continuously growing record of the community's identity, creativity, values, and its highest aspirations. Public art is defined as art placed on public or publicly-accessible private property, including building exteriors and outdoor public areas. Public art contributes to the local economy, and adds to the richness of the community. The consideration of public art should be included in every project's written development plan.

Public Art Guidelines

1. Public art should be available for the enjoyment and enrichment of people within the community. The plan should include a meaningful allowance for public art in the planning and construction of all projects.
2. Public art can include all mediums, such as sculpture, mural or painting, film, light, or other forms of creative expression that are viewable on a site or building. Commercial logos or elements of commercial advertising for a business or organization are not considered public art under this definition.
3. Public Art shall be designed, executed and supervised by recognized artists or other design professionals who have been trained or have consistently provided examples of artistic work in their medium of expression.
4. Public Art must be integrated with the design of every project or development, and shall aesthetically enhance the urban environment of the District.
5. Public Art proposals should conform to the design guidelines and master plan established by the Mayor's Committee for Public Art (MCOPA).

Site Planning, Parking Guidelines

Objective

Site Planning is an important part of any project. Your site should incorporate attractive and maintainable landscaping to enhance the hardscape of the building. Plants can be used in minimizing the visual impact of parking lot and service areas along BROD.

Site Planning, Parking Guidelines

1. Development Plans shall minimize the adverse visual impact of utility lines on the corridor. Underground lines or service from the alley, where feasible, is encouraged.
2. Combining existing small, under-utilized lots to create shared parking areas that are more efficient and more accessible is strongly encouraged.
3. Parking areas and Drive-Thru's should be located to the side or rear of structures.
4. Parking areas adjacent to the public sidewalks must use landscaping, trees, colonnades or other construction, to maintain the building line created by structures along the sidewalk.
5. Adequate perimeter landscaping, fencing, or a combination of both is required to help screen parked vehicles from full public view. The screening height should range between 30" and 36" above finished grade of parking lot. This height will enable drivers of vehicles to safely see and avoid other pedestrians and vehicles while

screening most of the parked vehicles mass.

6. New development projects should provide adequate and significant screening to adjacent residential structures. Opaque landscape buffers and other forms of screening shall be used to minimize noise and lighting impact.

7. Intensity, location, color, and direction of lighting shall be sensitive to nearby residential areas.

8. Fencing and screening shall be constructed of materials compatible with the principal structure.

9. Chain link fencing must not be visible from Bardstown Road/Baxter Avenue.

10. The number and width of curb-cuts on the corridor should be minimized to promote pedestrian circulation. Existing continuous curb-cuts should be reduced to widths necessary for vehicular traffic.

Historic Preservation Guidelines

Objective

Historic buildings (65 years of age or older) help to anchor the BROD to our community's history. Buildings can serve as reminders to future generations how Louisville's citizens lived and worked in the past while serving business and residential requirements today through adaptive reuse. These buildings can serve as future adaptive reuse opportunities. Contributing historical structures are structures that have unique designs, are constructed with unusual materials, or served the public in a manner that was important to the local area. Such structures also add character to the pattern of established development in the corridor. Given the significant role of historic structures in the corridor, demolition of any structure will entail stringent review.

Historic Preservation Guidelines

1. Changes to the exterior of Contributing Historical Structures and other structures within the corridor which were constructed in the last 65 years and that have not been significantly altered shall be reviewed in accordance with the standards established for Contributing Historic Structures by the United States Secretary of the Interior, but the Planning and Design Director or Committee may relax these standards in the interest of accomplishing the intentions of these Guidelines.

2. The design of new or substantially remodeled structures which are adjacent to Contributing Historic Structures should be compatible with them and should incorporate similar design details or references where appropriate.

3. No application to demolish any Contributing Historical Structure or structure built within the last 65 years shall be approved by the Urban Design Administrator unless

the applicant demonstrates to the satisfaction of Urban Design Administrator and the Historic Preservation Officer

- (a) That the rehabilitation of a Structure or construction of a new Structure will have a greater positive impact on the District's economic vitality and appearance than would preservation of the Structure proposed to be demolished and the rehabilitation of the Structure or the construction of the new Structure would not be possible or economically feasible without the demolition of the Structure proposed to be demolished; or
- (b) That the applicant cannot obtain a reasonable economic return from the property or Structure unless the Contributing Historical Structure or Structure constructed within the last 65 years is demolished in accordance with the application.